

STATE OF ALASKA

DEPT. OF ENVIRONMENTAL CONSERVATION

DIVISION OF WATER

Wastewater Discharge Authorization Program

SEAN PARNELL, GOVERNOR

555 Cordova Street
Anchorage, AK 99501-2617

Phone: (907) 269-7564

Fax: (907) 334-2415

TTY: (907) 269-7511

<http://www.state.ak.us/dec/>

October 20, 2010

Certified Mail 7008-1830-0003-5208-0249

City Manager
Municipality of Skagway
P.O. Box 415
Skagway, AK 99840

Subject: Taiya Inlet, Municipality of Skagway, Sheet Pile Bulkhead
Reference No. POA-2009-1049
State ID No. AK 1008-03J

Dear City Manager:

In accordance with Section 401 of the Federal Clean Water Act of 1977 and provisions of the Alaska Water Quality Standards, the Department of Environmental Conservation is issuing the enclosed Certificate of Reasonable Assurance for the construction of a sheet pile bulkhead.

Department of Environmental Conservation (DEC) regulations provide that any person who disagrees with this decision may request an informal review by the Division Director in accordance with 18 AAC 15.185 or an adjudicatory hearing in accordance with 18 AAC 15.195 – 18 AAC 15.340. An informal review request must be delivered to the Director, Division of Water, 555 Cordova Street, Anchorage, AK 99501, within 15 days of the permit decision. Visit <http://www.dec.state.ak.us/commish/ReviewGuidance.htm> for information on Administrative Appeals of Department decisions.

An adjudicatory hearing request must be delivered to the Commissioner of the Department of Environmental Conservation, 410 Willoughby Avenue, Suite 303, PO Box 111800, Juneau, AK 99811-1800, within 30 days of the permit decision. If a hearing is not requested within 30 days, the right to appeal is waived.

By copy of this letter we are advising the U.S. Army Corps of Engineers and Division of Coastal and Oceans Management of our actions and enclosing a copy of the certification for their use.

Sincerely,



Sharon Morgan, Manager

Wastewater Discharge Authorization Program

Enclosure

cc: (with encl.)

Randal Vigil, USACE, Juneau

Tracy DeGering, EPA, AK Operations

Jill Taylor, DNR/DCOM, Juneau

Sean Palmer, ADEC Anchorage

Katie Eaton, ADF&G/Habitat, Juneau

Laura Gurley, PND Engs., Inc., Seattle

U.S. Fish & Wildlife Service, JNU Field Office

STATE OF ALASKA
DEPARTMENT OF ENVIRONMENTAL CONSERVATION
CERTIFICATE OF REASONABLE ASSURANCE

A Certificate of Reasonable Assurance, in accordance with Section 401 of the Federal Clean Water Act and the Alaska Water Quality Standards, is issued to the Municipality of Skagway, P.O. Box 415, Skagway, AK 99840 for the construction of a sheet pile bulkhead. The applicant proposes to discharge approximately 100,000 cubic yards of dredge and/or fill material into approximately 1.8 acres below the plane of the high tide line (approximate elevation +21.2 feet above the 0.0 foot contour) contained by an 800 lineal foot steel sheet pile bulkhead installed below the plane of mean high water (MHW) (approximate elevation +15.8 feet above the 0.0 foot contour) in Taiya Inlet. The proposed work below the HTL will include the excavation of approximately 1,080 cubic yards of rock riprap. The applicant proposes to reinstall the rock at the proposed bulkhead wall terminations down to approximately -10 feet MLLW and discharge rock within the bulkhead area. The applicant also propose to install seven fenders along the face of the bulkhead, which will consist of two steel pipe pin piles (20-inch diameter) and a fender panel with rubber energy absorbers.

The proposed activity is located within Section 14, T. 28 S., R. 59 E., Copper River Meridian in Skagway, Alaska.

Public notice of the application for this certification was given as required by 18 AAC 15.180.

Water Quality Certification is required under Section 401 because the proposed activity will be authorized by a U.S. Army Corps of Engineers permit, reference number POA-2009-1049, and a discharge may result from the proposed activity.

DEC reviewed the application and certifies that there is reasonable assurance that the proposed activity, as well as any discharge which may result, will comply with applicable provisions of Section 401 of the Clean Water Act and the Alaska Water Quality Standards, 18 AAC 70, provided that the following alternative measures are adhered to.

1. Reasonable precautions and controls must be used to prevent incidental and accidental discharge of petroleum products or other hazardous substances. Fuel storage and handling activities for equipment must be sited and conducted so there is no petroleum contamination of the ground, surface runoff or water bodies.
2. During construction, spill response equipment and supplies such as sorbent pads shall be available and used immediately to contain and cleanup oil, fuel, hydraulic fluid, antifreeze, or other pollutant spills. For any spill amount, there are Discharge Notification and Reporting Requirements (AS 46.03.755 and 18 AAC 75 Article 3). Most importantly, the applicant must contact by

telephone the DEC Area Response Team for Southeast Alaska at (907) 465-5340 during work hours or 1-800-478-9300 after hours. Also, the applicant must contact by telephone the National Response Center at 1-800-424-8802. Report all spills.

3. Construction equipment shall not be operated below the ordinary high water mark if equipment is leaking fuel, oil, hydraulic fluid, or any other hazardous material. Operation of tracked or wheeled equipment in the water shall be kept to a minimum. Equipment shall be inspected on a daily basis for leaks. If leaks are found, the equipment shall not be used and shall be pulled from service until the leaks are repaired.
4. Fill materials must consist of rock fill and riprap that are free from fines and suspendible material to the extent practicable, and free from contamination by petroleum products or toxic substances.

This certification expires five (5) years after the date the certification is signed. If your project is not completed by then and work under Corps of Engineers Permit will continue, you must submit an application for renewal of this certification no later than 30 days before the expiration date (18 AAC 15.100).

Date Oct 20, 2010

William Ashton for
Sharon Morgan, Manager
Wastewater Discharge Authorization Program



US Army Corps
of Engineers
Alaska District

Public Notice of Application for Permit

Juneau Field Office
Regulatory Division (1145)
CEPOA-RD
8800 Glacier Highway, Suite 106
Juneau, Alaska 99801-8079

PUBLIC NOTICE DATE: July 30, 2010
EXPIRATION DATE: August 30, 2010
REFERENCE NUMBER: POA-2009-1049
WATERWAY: Taiya Inlet

Interested parties are hereby notified that a Department of the Army permit application has been received for work in waters of the United States as described below and shown on the enclosed project drawings.

Comments on the described work, with the reference number, should reach this office no later than the expiration date of this Public Notice to become part of the record and be considered in the decision. Please contact **Mr. Randal P. Vigil** at (907) 790-4491, or by email at **Randal.P.Vigil@usace.army.mil** if further information is desired concerning this notice.

APPLICANT: Municipality of Skagway, Post Office Box 415, Skagway, Alaska 99840.

AGENT: PND Engineers, Inc., 811 First Avenue, Suite 570, Seattle, Washington 98104.

LOCATION: The project site is located within the SE ¼ NE ¼ NW ¼ Section 14, T. 28 S., R. 59 E., Copper River Meridian; USGS Quad Map Skagway B-1; Latitude 59.450° N., Longitude -135.331° W.; Skagway Ore Terminal Facility, 906 South Terminal Way, in Skagway, Alaska.

PURPOSE: The applicant's stated purpose is to "expand the capacity of the marine facility in order to accommodate increased demand for port facilities in the Skagway Harbor for cargo, passengers, ore and other marine transported materials."

PROPOSED WORK: The applicant requests authorization to perform work in waters and navigable waters of the United States to construct a 340 feet long by 50 feet wide pile supported dock with sheet pile bulkhead under a two phase project schedule.

Phase 1 would consist of demolition of some existing facilities to allow for the construction of a new filled sheet pile bulkhead and new ship loader while maintaining mooring function for cruise ships and ore ships. The work performed under this phase would consist of the following:

1. Discharge approximately 100,000 cubic yards of dredged and/or fill material into approximately 1.8 acres below the plane of the high tide line (approximate elevation +21.2 feet above the 0.0 foot contour) contained by an 800 lineal foot steel sheet pile bulkhead installed below the plane of mean high water (approximate

elevation +15.8 feet above the 0.0 foot contour) (MHW) in Taiya Inlet. Work below the HTL would include the excavation of approximately 1,080 cubic yards of rock riprap. The rock would be reinstalled at the proposed bulkhead wall terminations down to approximately -10 feet MLLW and discharged within the bulkhead area. Seven fenders would be installed along the face of the bulkhead, which would consist of 2 steel pipe pin plies (20-inch diameter) and a fender panel with rubber energy absorbers.

2. Demolish and remove from below MHW approximately 38,200 square feet of the concrete and timber, ore dock. Demolition would include removal of approximately 440 piles total (approximately 70 steel pipe piles and 370 timber piles) using vibratory methods. There are currently two dolphins (at least 3 steel piles each) located west of and alongside the south face of the ore dock. These dolphins would be removed using a vibratory extractor operated from a barge mounted crane. All associated catwalks, utilities, and other dock appurtenances would also be removed. Timber piles that break during extraction attempts may be cut 2 feet below the mudline.

3. Two temporary catwalks would be installed below MHW for use during Phase 1. One catwalk (60 feet long) would connect the proposed bulkhead to the existing concrete dock to the south and the other (45 feet long) would connect the proposed bulkhead to the existing catwalk system to the north. This would allow for ease of access to existing mooring points during ship berthing. Both of these catwalks would be removed and replaced during Phase 2.

4. Install below the plane of MHW, 1 ore loader at the south end of the proposed sheet pile bulkhead fill. The loader towers would be mounted on CIP pile caps supported by approximately 20 steel piles (30- inch diameter), each installed within the proposed bulkhead fill. A ship loading conveyor would be mounted on top of the towers.

Phase 2 would consist of demolition of additional existing facilities to allow for the construction of new dolphins and catwalks and a new extension of the existing concrete platform dock south of the existing ore loader. This phase consists of the following:

1. Remove from below MHW one dolphin furthest south of the existing concrete platform dock, including the catwalks connecting the dock and five existing dolphins. Remove from below MHW 3,300 square feet of existing concrete ore dock, two adjacent dolphins to the south of the proposed bulkhead and 4,000 square feet of timber dock and catwalk located to the north of the proposed bulkhead, including all associated catwalks, utilities, and other dock appurtenances. Demolition would include removal of approximately 90 piles total (50 steel pipe piles and 40 timber piles) using vibratory methods. Timber piles that break during extraction attempts may be cut 2 feet below the mudline.

2. Install below MHW a 50 feet wide by 340 feet long, pile-supported (approximately 64 steel, 30- and 24-inch diameter), precast concrete decked dock extension south of the existing concrete platform dock.

3. Install below MHW three steel-pile dolphins (5 piles, 48-inch diameter per structure) connected by steel catwalks south of the new concrete dock extension. Fenders consisting of two steel pipe pin piles (30-inch diameter) and a fender panel with rubber energy absorbers would be installed on the face of two of the dolphins.

Install three steel-pile breasting dolphins (3 piles, 30-inch diameter per structure) connected via catwalk between the White Pass Yukon Route floating dock and the proposed bulkhead. Fenders would be installed on the face of each dolphin consisting of two steel pipe pin piles (30-inch diameter) and a fender panel with rubber energy absorbers.

Install below MHW two new steel pile breasting dolphins (3 piles, 30-inch steel per structure) connected via catwalk between the proposed bulkhead and the shore. Fenders would be installed on the face of each dolphin consisting of two steel pipe pin piles (30-inch diameter) and a fender panel with rubber energy absorbers.

All work would be performed in accordance with the enclosed plan (sheets 1-17), dated June 2010.

ADDITIONAL INFORMATION: The removed dolphins in whole or in part may be salvaged for use elsewhere. Some steel piles may be salvaged for use on another municipal project, and all other materials would be reused or disposed of off-site and in accordance with regulations.

Sheet and pipe piles would be driven using a vibratory and/or impact hammer from a barge mounted crane.

Dredged fill material would consist of the marine sediment from the proposed Skagway Small Boat Harbor Expansion project (a separate Municipality of Skagway project not yet underway but would be coordinated with this project) in an effort to avoid in-water disposal. The applicant anticipates that harbor dredging would yield approximately 62,000 cubic yards of material or approximately 125,000 cubic yards, if the small boat harbor is expanded. Additional borrow material would be supplemented as needed, and would be procured by the contractor from an approved source, likely an upland quarry.

PND Engineers, Inc. have submitted to the U.S. Army Corps of Engineers, Alaska District (Corps) a sediment characterization and analysis report dated June 2008, for the Skagway Small Boat Harbor proposed dredging. This sediment characterization study was performed under a sediment sampling plan approved on April 14, 2008, by the Corps. Based on the results of the report, the Corps has determined that material dredged from the Skagway Small Boat Harbor would be suitable for confined disposal. As of this date, no DA permit application for the Skagway Small Boat Harbor dredging has been submitted and no Corps authorization has been granted.

A crane would be installed on the proposed structure so materials can be on and offloaded directly onto either rail or semi-tractor trailer. Rail service would be extended from the east onto the proposed dock (four sets of tracks). This configuration would allow for additional moorage and unloading space for shipping containers and other bulk and large cargo.

APPLICANT PROPOSED MITIGATION: The applicant has provided the following statements proposing mitigation measures to avoid, minimize, and compensate for impacts to waters of the United States from activities involving discharges of dredged or fill material.

a. Avoidance: "The sediments adjacent to the existing ore dock tested positive for sediment metal concentrations, including copper, cadmium and nickel most of which were below NOAA ER-M and WA sediment quality guidelines with the exception of certain locations where zinc and lead levels were higher. The same area also tested positive for elevated surface water and sediment petroleum derivatives and total organic carbon levels. These contaminants are likely a result of years of historic metal ore transport and loading operations. In order to minimize disturbance of this material during construction of the new facility, a sheet pile wall design option was considered. This option offers the opportunity to isolate and encapsulate the portion of the known affected area by containing it within the footprint of the wall under the fill material. The sheet pile design feature would prevent the affected materials from migrating to clean areas, affecting water quality, and would avoid harmful impacts to aquatic flora and fauna."

b. Minimization: "In order to minimize the unavoidable impacts, Best Management Practices (BMPs) would be employed to minimize impacts on water quality, fish life, and the environment during construction. Such practices include:

1. Impact driving would be required on all bearing piles. Vibratory methods may be used for initial setting and driving of all of the piles. Fender pin piles may be installed using a vibratory hammer exclusively, if possible.
2. A containment curtain would be deployed around all areas where excavation or in-water fill placement work would occur.
3. Timing restrictions, which specify that in-water work must occur when juvenile salmonids are absent or present in very low numbers, would be strictly observed.
4. The contractor would be required to capture any falling debris associated with project construction; such methods would not allow any debris to enter the Taiya Inlet.
5. Excess or waste materials would not be allowed to enter waters. All such materials would be collected and recycled or disposed of at an approved facility.
6. The contractor would comply with water quality restrictions as required by law and implement corrective measures if temporary water quality standards are exceeded.
7. Care would be taken to prevent any petroleum products or other toxic or deleterious materials from entering the waters. Fuel hoses, oil drums, oil or fuel transfer valves and fittings, etc., would be checked regularly for drips or leaks, and shall be maintained and stored properly on secondary containment pallets to prevent spills.
8. The contractor would have a spill kit with oil-absorbent materials on site to be used in the event of a spill or if any oil product is observed in the water.

The contractor is responsible for the preparation of a Spill Prevention, Control, and Countermeasures (SPCC) plan to be used for the duration of the project if required by permitting agencies.

Compared to the prior submitted concept the decrease in filled area and volume would decrease the potential environmental impact of the project to fish and other wildlife as well as decrease the amount of affected navigable waters of the United States."

c. Compensatory Mitigation: "The current design results in very little loss of useful habitat. The area to be filled is located well within the manmade harbor area that currently consists of riprap slopes and pile supported timber and concrete docks. This is not high value habitat, so placing fill here would be less intrusive. The existing rip rap shore protection under the dock does not provide value to the aquatic ecosystem. As such, no compensatory mitigation is proposed."

WATER QUALITY CERTIFICATION: A permit for the described work would not be issued until a certification or waiver of certification, as required under Section 401 of the Clean Water Act (Public Law 95-217), has been received from the Alaska Department of Environmental Conservation.

COASTAL ZONE MANAGEMENT ACT CERTIFICATION: Section 307(c)(3) of the Coastal Zone, Management Act of 1972, as amended by 16 U.S.C. 1456(c)(3), requires the applicant to certify the described activity affecting land or water uses in the Coastal Zone complies with the Alaska Coastal Management Program. A permit would not be issued

until the Division of Coastal and Ocean Management, Department of Natural Resources has concurred with the applicant's certification.

CULTURAL RESOURCES: The latest published version of the Alaska Heritage Resources Survey (AHRS) has been consulted for the presence or absence of historic properties, including those listed in or eligible for inclusion in the National Register of Historic Places. There are no listed or eligible properties in the vicinity of the worksite. Consultation of the AHRS constitutes the extent of cultural resource investigations by the District Commander at this time, and he is otherwise unaware of the presence of such resources. This application is being coordinated with the State Historic Preservation Office (SHPO). Any comments SHPO may have concerning presently unknown archeological or historic data that may be lost or destroyed by work under the requested permit will be considered in our final assessment of the described work.

ENDANGERED SPECIES: The project area is within the known or historic range of the Humpback whale (*Megaptera novaeangliae*). Preliminarily, we have determined the described activity may affect but is not likely to adversely affect the endangered Humpback whale. We have also preliminarily determined the activity is not likely to adversely affect the species' designated critical habitat, under the Endangered Species Act of 1973 (87 Stat. 844).

This application is being coordinated with the U.S. Fish and Wildlife Service and the National Marine Fisheries Service (NMFS). Any comments they may have concerning endangered or threatened wildlife or plants or their critical habitat will be considered in our final assessment of the described work.

ESSENTIAL FISH HABITAT: The Magnuson-Stevens Fishery Conservation and Management Act, as amended by the Sustainable Fisheries Act of 1996, requires all Federal agencies to consult with the NMFS on all actions, or proposed actions, permitted, funded, or undertaken by the agency, that may adversely affect Essential Fish Habitat (EFH).

The project area is within the known range of Chum salmon (*Oncorhynchus keta*), Coho salmon (*Oncorhynchus kisutch*), and Pink salmon (*Oncorhynchus gorbuscha*) and Eulachon (*Thaleichthys pacificus*).

Preliminarily, we have determined the described activity would not adversely affect EFH in the project area.

TRIBAL CONSULTATION: The Alaska District fully supports tribal self-governance and government-to-government relations between Federally recognized Tribes and the Federal government. Tribes with protected rights or resources that could be significantly affected by a proposed Federal action (e.g., a permit decision) have the right to consult with the Alaska District on a government-to-government basis. Views of each Tribe regarding protected rights and resources will be accorded due consideration in this process. This Public Notice serves as notification to the Tribes within the area potentially affected by the proposed work and invites their participation in the Federal decision-making process regarding the protected Tribal right or resource. Consultation may be initiated by the affected Tribe upon written request to the District Commander during the public comment period.

PUBLIC HEARING: Any person may request, in writing, within the comment period specified in this notice, that a public hearing be held to consider this application. Requests for public hearings shall state, with particularity, reasons for holding a public hearing.

EVALUATION: The decision whether to issue a permit will be based on an evaluation of the probable impacts including cumulative impacts of the proposed activity and its intended use on the public interest. Evaluation of the probable impacts, which the proposed activity may have on the public interest, requires a careful weighing of all the factors that become relevant in each particular case. The benefits,

which reasonably may be expected to accrue from the proposal, must be balanced against its reasonably foreseeable detriments. The outcome of the general balancing process would determine whether to authorize a proposal, and if so, the conditions under which it will be allowed to occur. The decision should reflect the national concern for both protection and utilization of important resources. All factors, which may be relevant to the proposal, must be considered including the cumulative effects thereof. Among those are conservation, economics, aesthetics, general environmental concerns, wetlands, cultural values, fish and wildlife values, flood hazards, floodplain values, land use, navigation, shore erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food and fiber production, mineral needs, considerations of property ownership, and, in general, the needs and welfare of the people. For activities involving 404 discharges, a permit will be denied if the discharge that would be authorized by such permit would not comply with the Environmental Protection Agency's 404(b)(1) guidelines. Subject to the preceding sentence and any other applicable guidelines or criteria (see Sections 320.2 and 320.3), a permit will be granted unless the District Commander determines that it would be contrary to the public interest.

The Corps of Engineers is soliciting comments from the public; Federal, State, and local agencies and officials; Indian Tribes; and other interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the Corps of Engineers to determine whether to issue, modify, condition or deny a permit for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act. Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.

AUTHORITY: This permit will be issued or denied under the following authorities:

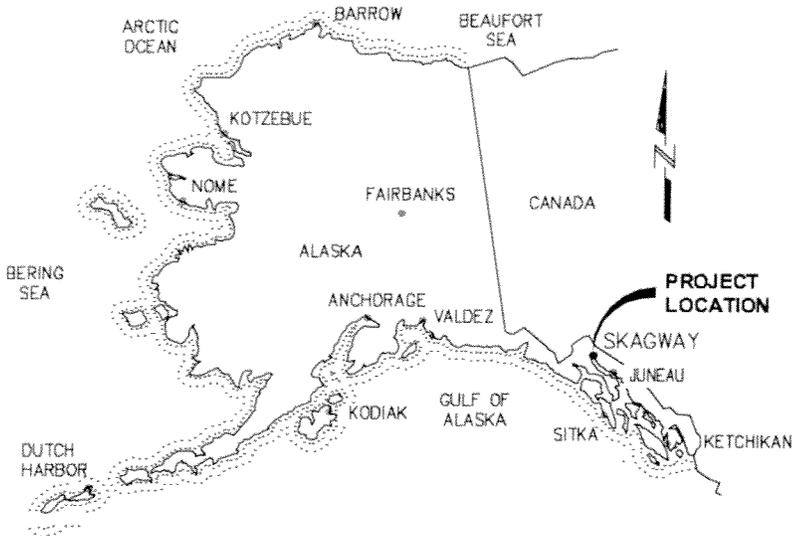
(X) Perform work in or affecting navigable waters of the United States - Section 10 Rivers and Harbors Act 1899 (33 U.S.C. 403).

(X) Discharge dredged or fill material into waters of the United States - Section 404 Clean Water Act (33 U.S.C. 1344). Therefore, our public interest review will consider the guidelines set forth under Section 404(b) of the Clean Water Act (40 CFR 230).

Project drawings, Notice of Application for Certification of Consistency with the Alaska Coastal Management Program, and Notice of Application for State Water Quality Certification are enclosed with this Public Notice.

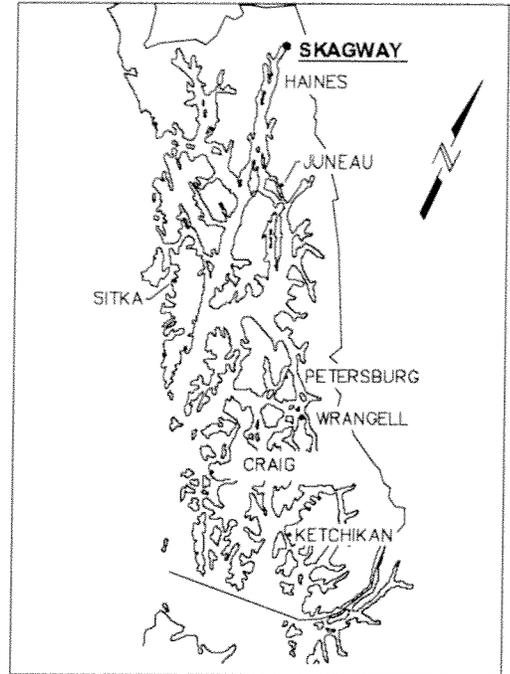
District Commander
U.S. Army, Corps of Engineers

Enclosures



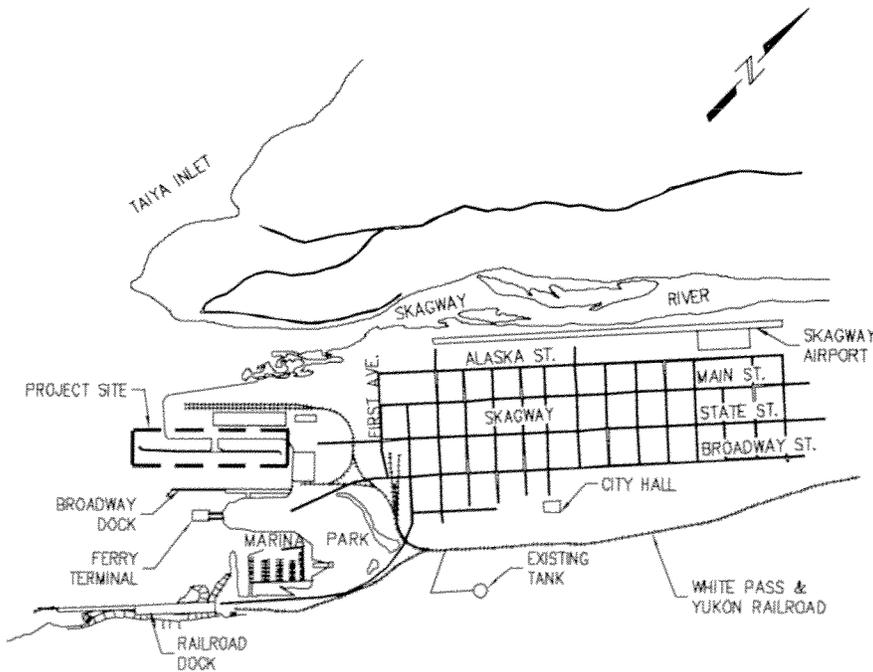
VICINITY MAP

NOT TO SCALE



SOUTHEAST ALASKA

SCALE: 1 IN. = 170 MILES (APPROX.)



SITE MAP

NOT TO SCALE

TIDAL INFORMATION

	ELEVATION (ft)
HIGHEST OBSERVED (10/21/1945)	+25.8
HTL	+21.2'
MHHW	+16.7
MHW	+15.8
MLW	+1.6
MLLW	0.0
LOWEST OBSERVED (1/1/1991)	-6.0

Lat. 59.452', Long. 135.324'

PURPOSE:
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

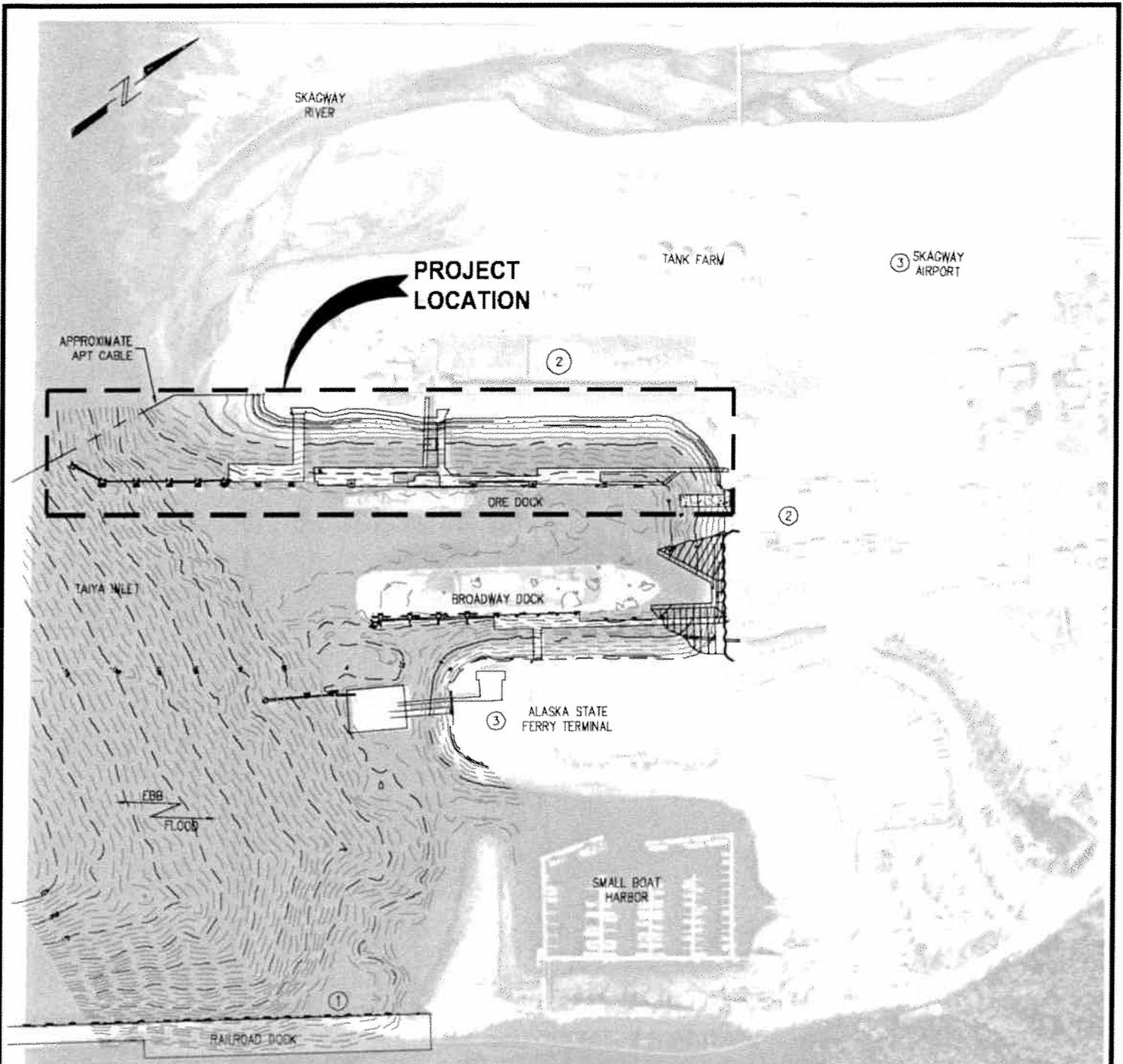
1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 VICINITY MAP
 AND SITE MAP**

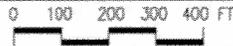
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 1 of 17 DATE: JUNE 2010



EXISTING SITE PLAN



PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

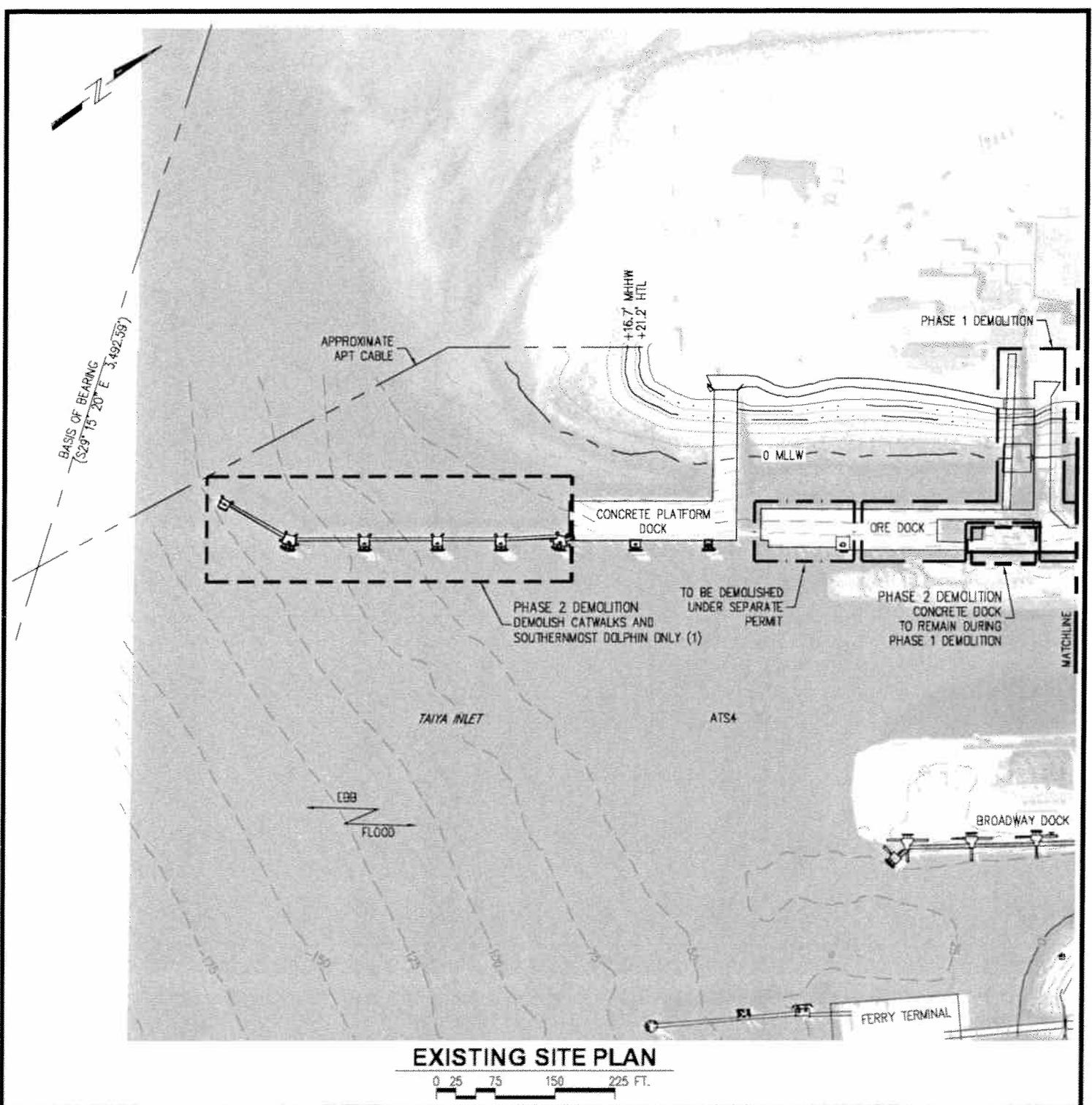
**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 EXISTING
 SITE PLAN**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 2 of 17 DATE: JUNE 2010

Drawings \2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01-P3-P4.dwg



PURPOSE:
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 DEMOLITION PLAN**

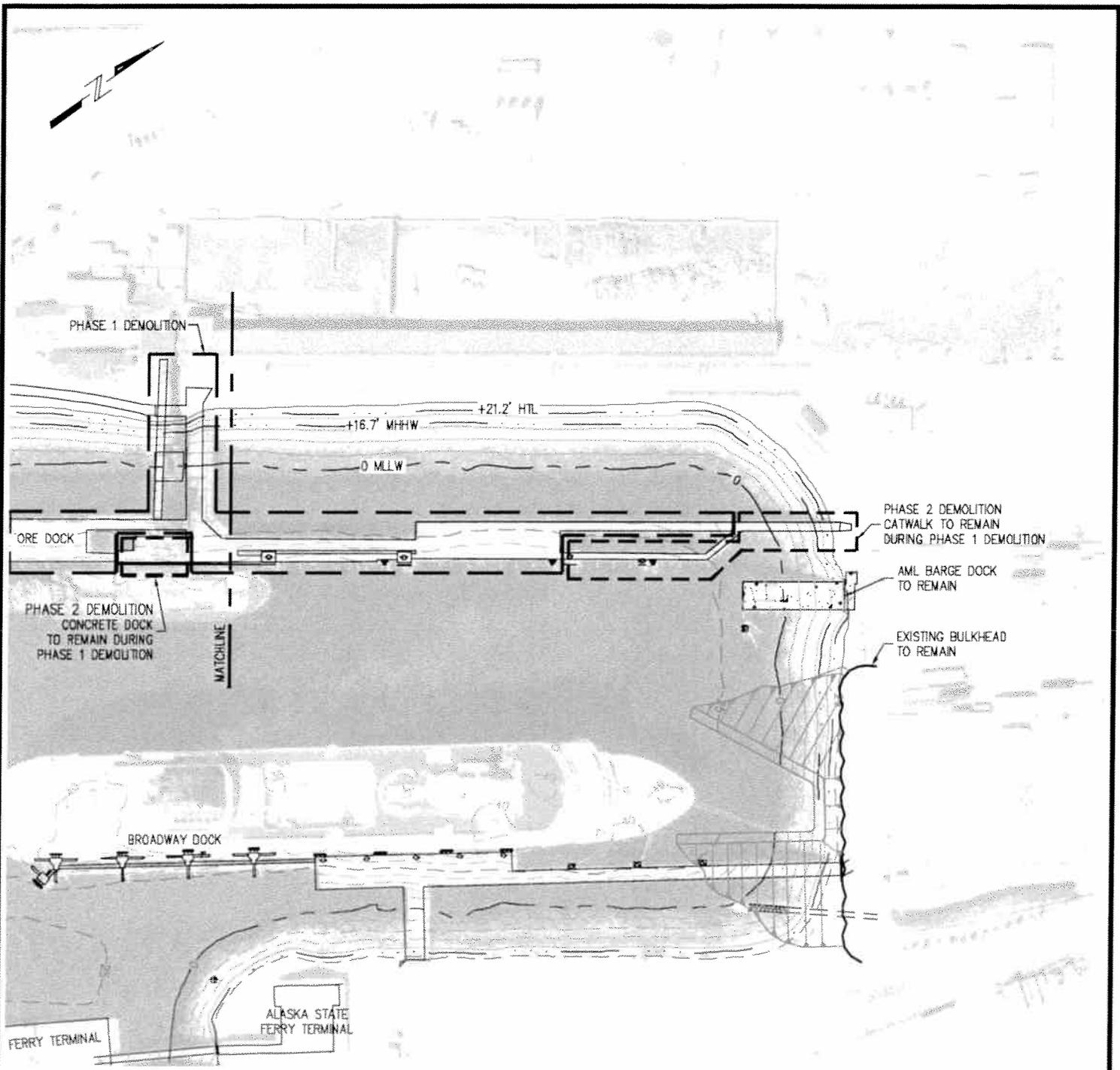
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E

SHEET 3 of 17 DATE: JUNE 2010

Drawings\2009\09-4050.01 - Skagway Gateway Project\Permit 2010\094050.01--P3--P4.dwg



EXISTING SITE PLAN



PURPOSE:
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 DEMOLITION PLAN**

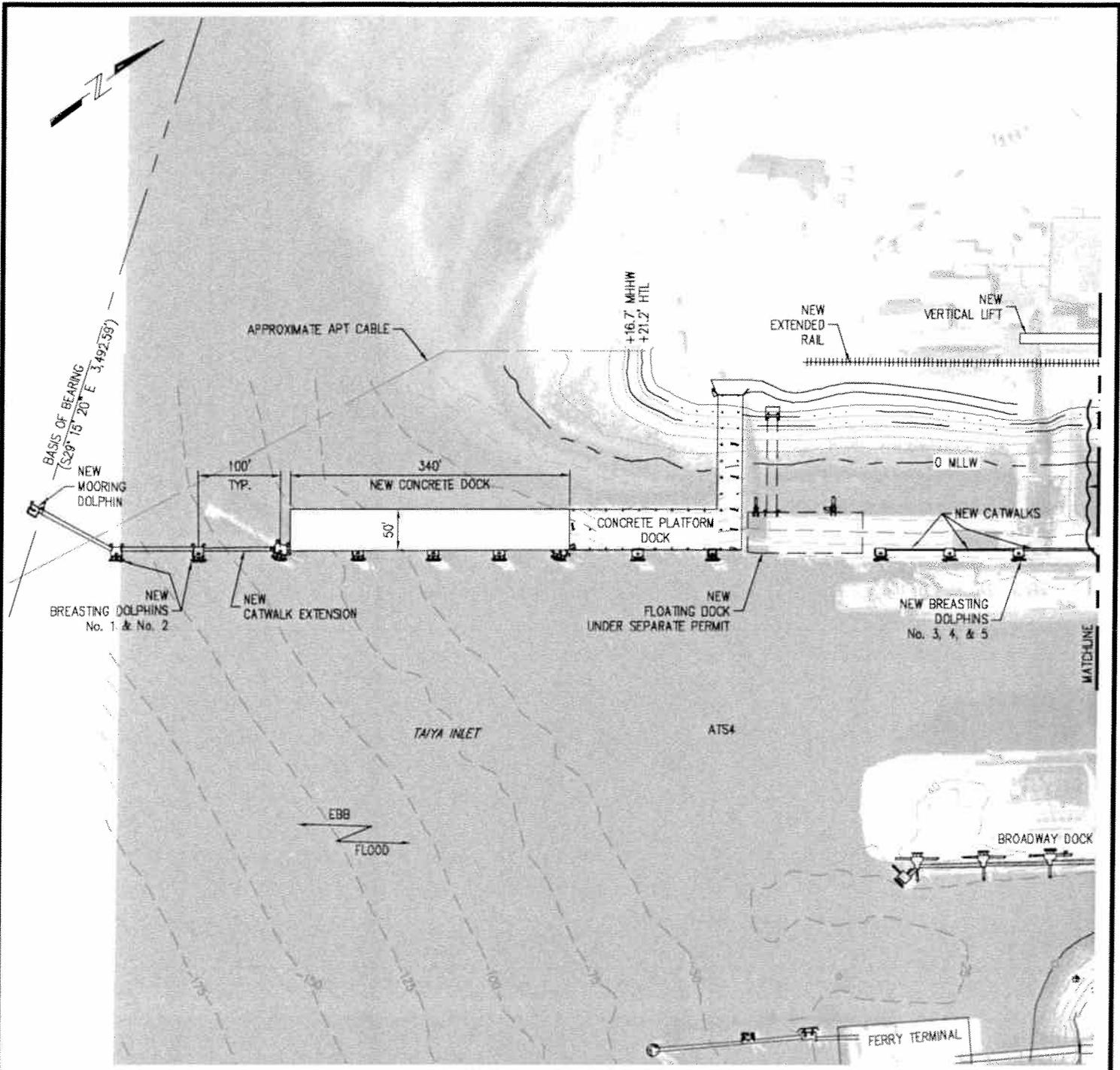
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E

SHEET 4 of 17 DATE: JUNE 2010

Drawings \2009\09-4050-01 - Skagway Gateway Project\Permit 2010\09-4050-01-P5-P6.dwg



EXISTING SITE PLAN

0 25 75 150 225 FT.

PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

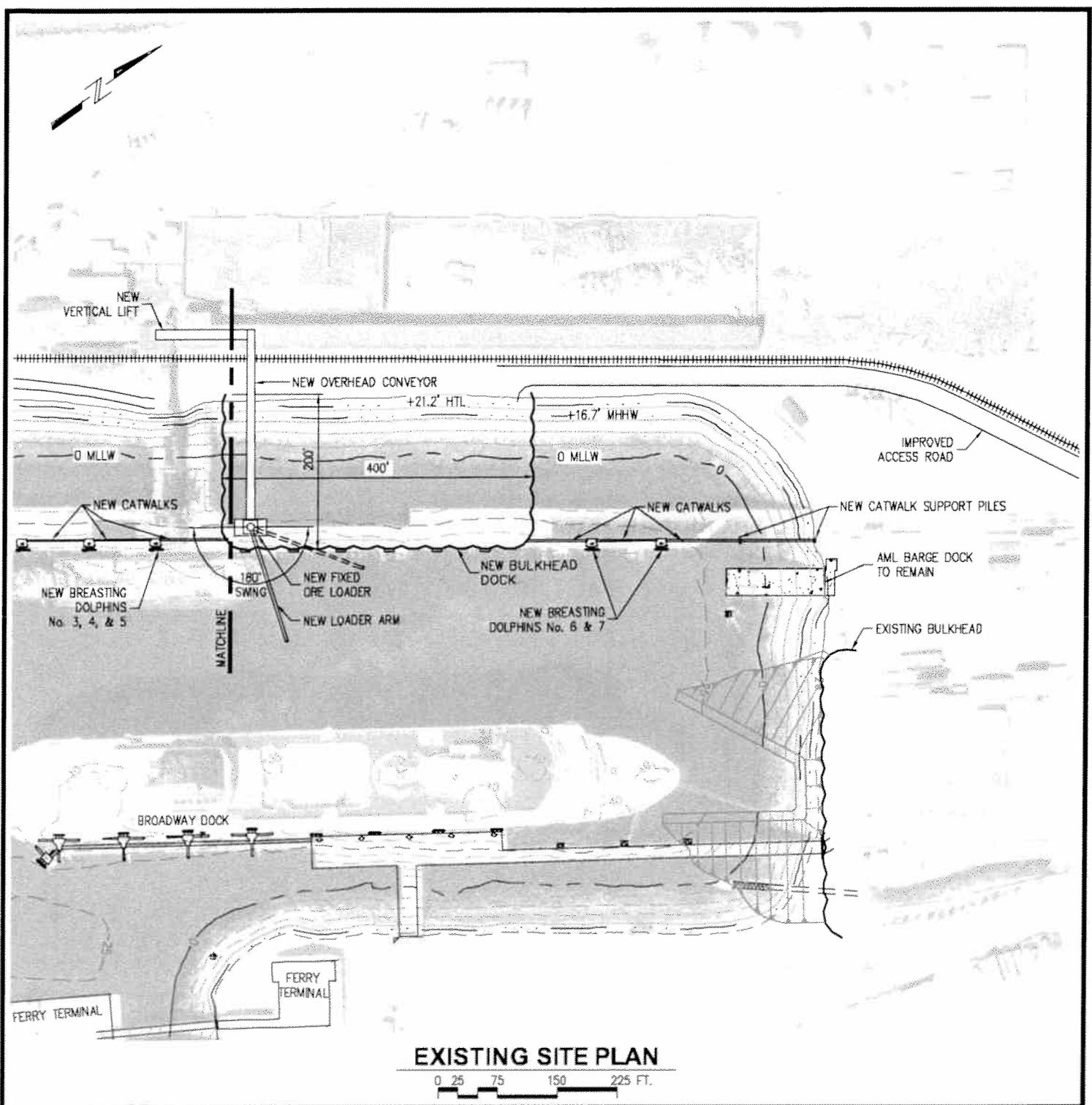
**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PROPOSED
 FINAL SITE PLAN**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 5 of 17 DATE: JUNE 2010

Drawings\2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - P5-P6.dwg



EXISTING SITE PLAN

0 25 75 150 225 FT.

PURPOSE:
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PROPOSED
 FINAL SITE PLAN**

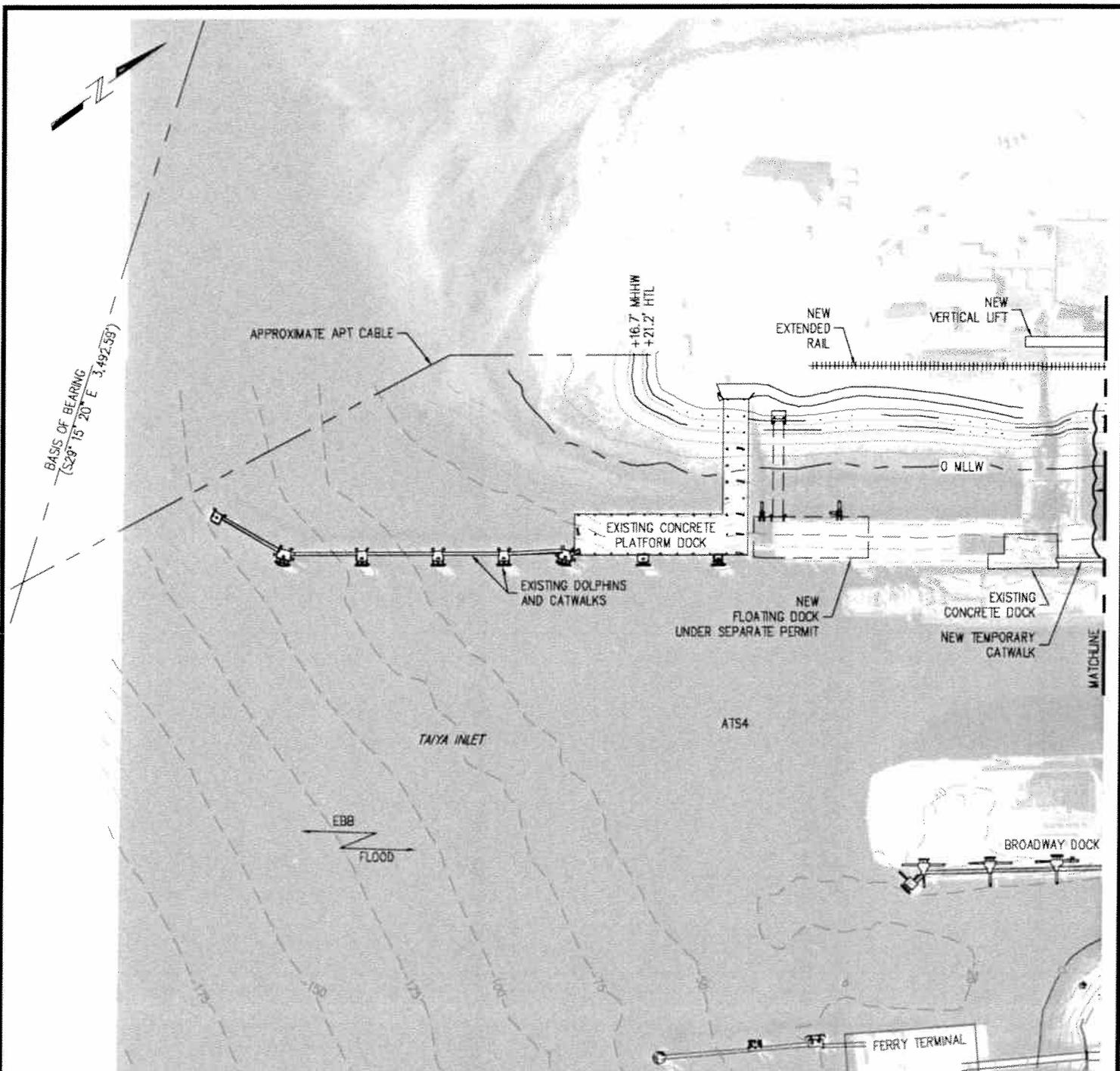
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E

SHEET 6 of 17 DATE: JUNE 2010

Drawings\2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01-P7-P8.dwg



SITE PLAN



PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

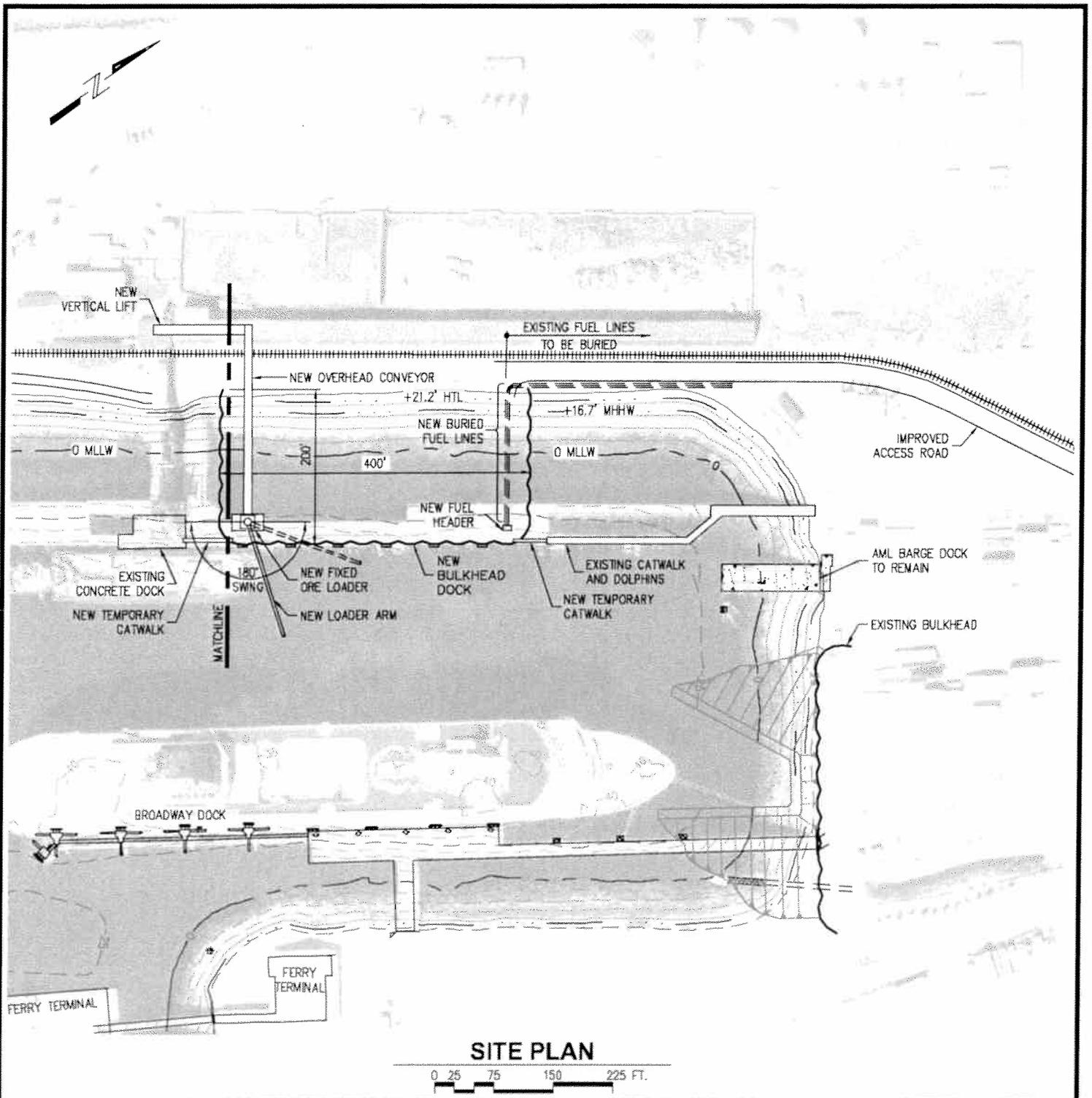
1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE I
 CONSTRUCTION**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 7 of 17 DATE: JUNE 2010



SITE PLAN

0 25 75 150 225 FT.

PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
 (RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 1
 CONSTRUCTION**

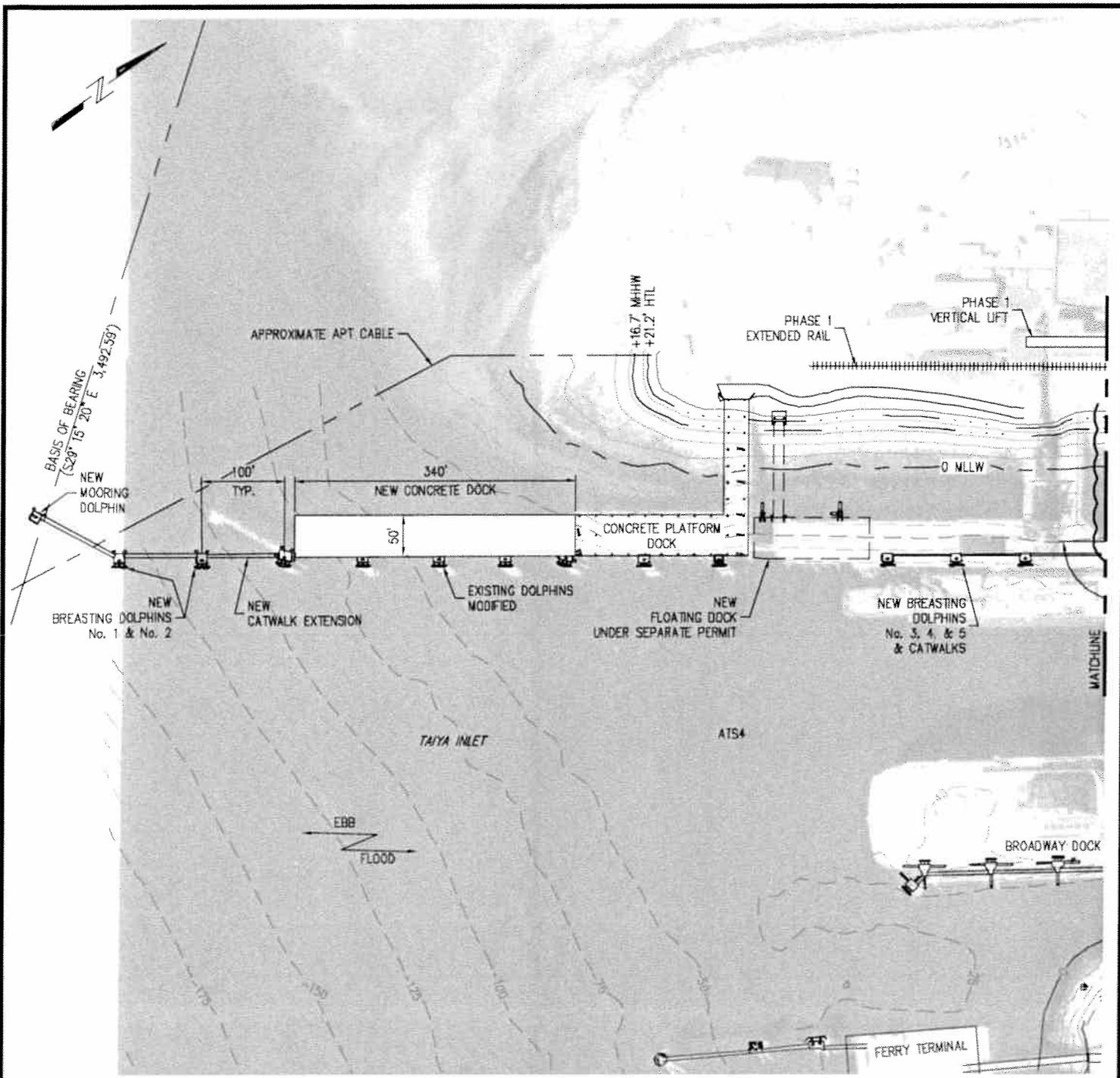
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 8 of 17 DATE: JUNE 2010

Drawings\2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - P7 - PB.dwg

Drawings \2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - P9-P10.dwg



SITE PLAN
 0 25 75 150 225 FT.

PURPOSE:
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
 (RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 2
 CONSTRUCTION**

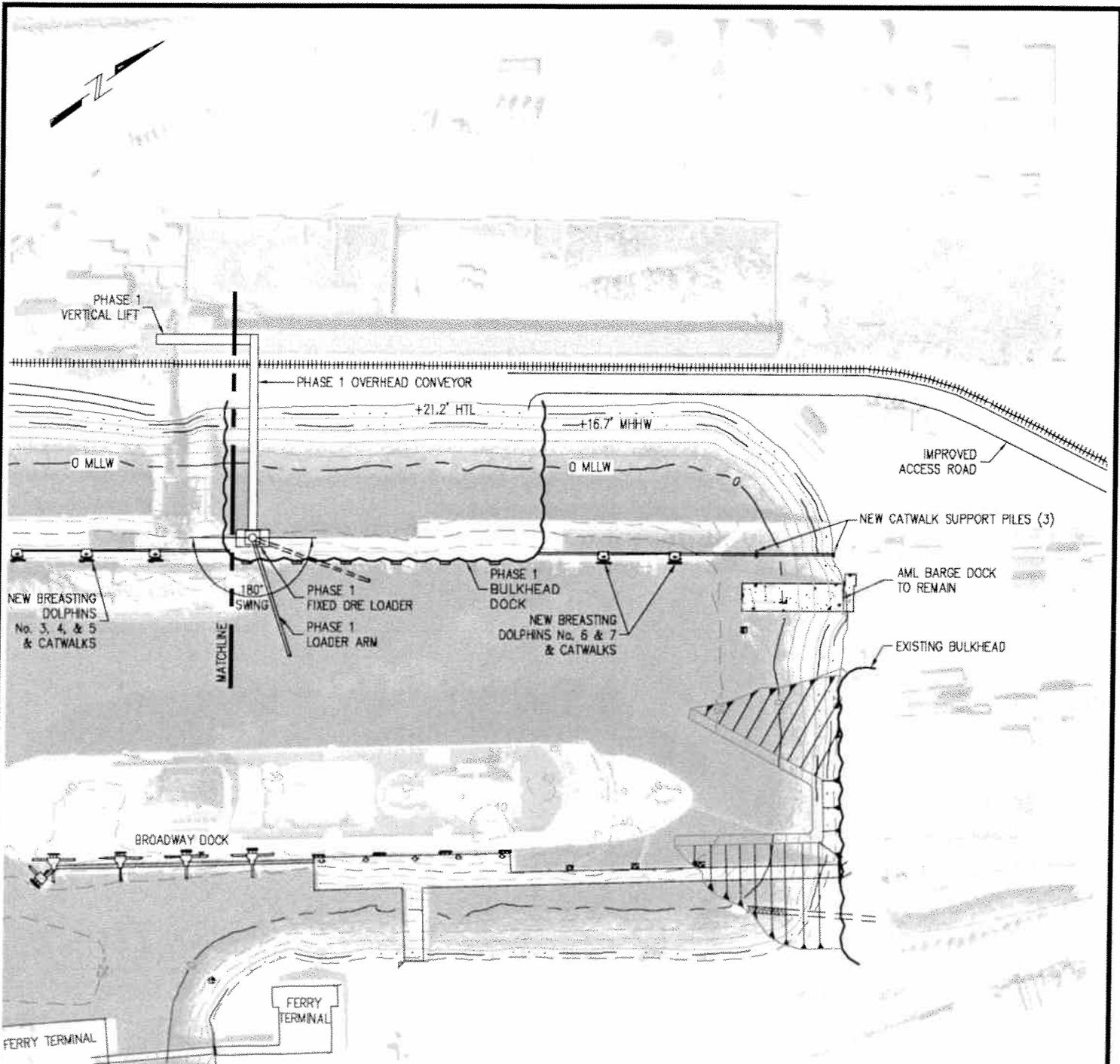
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E

SHEET 9 of 17 DATE: JUNE 2010

Drawings\2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - P9 - P10.dwg



SITE PLAN

0 25 75 150 225 FT.

PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

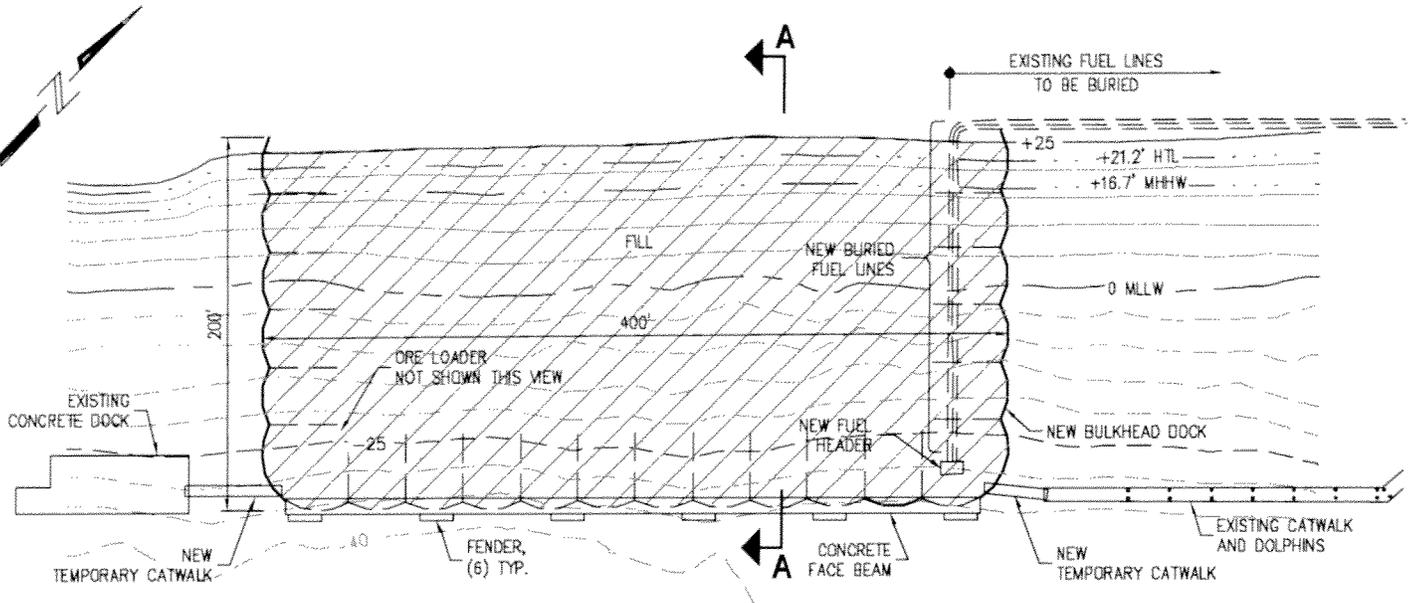
1. SKAGWAY TERMINAL CO.
 (RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 2
 CONSTRUCTION**

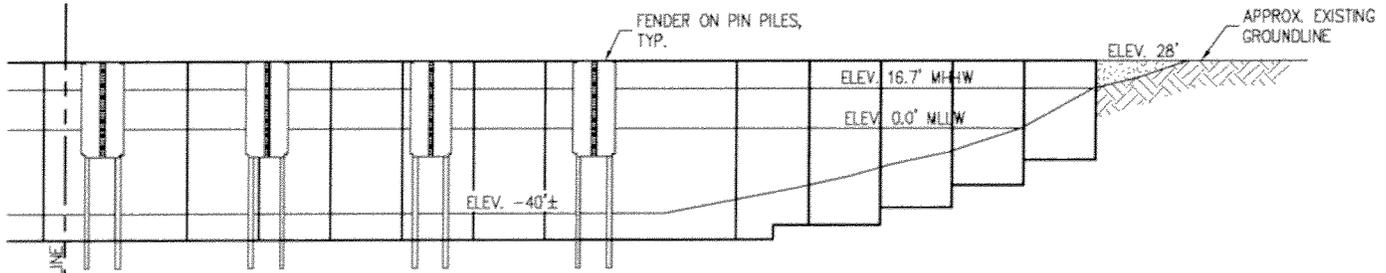
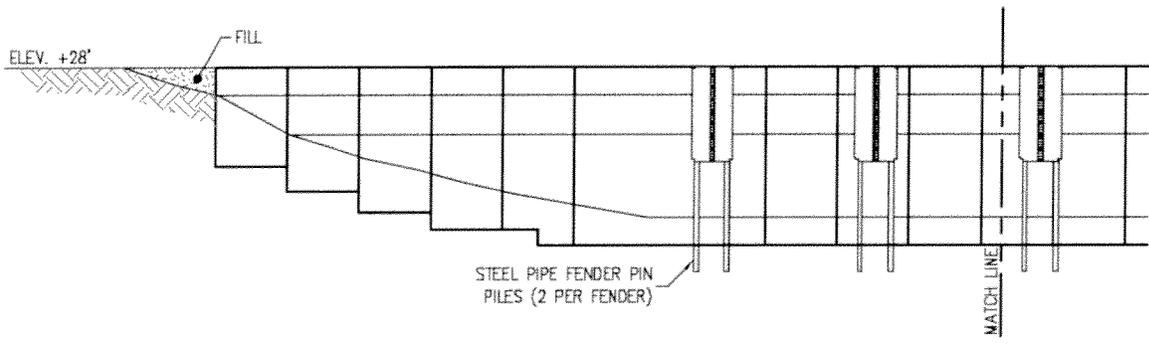
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 10 of 17 DATE: JUNE 2010



PLAN



ELEVATION

NOTE:
ALL PIPE PILE IS STEEL

PURPOSE:
PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO. (RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

YUKON GATEWAY REVISED SKAGWAY, ALASKA

PHASE 1

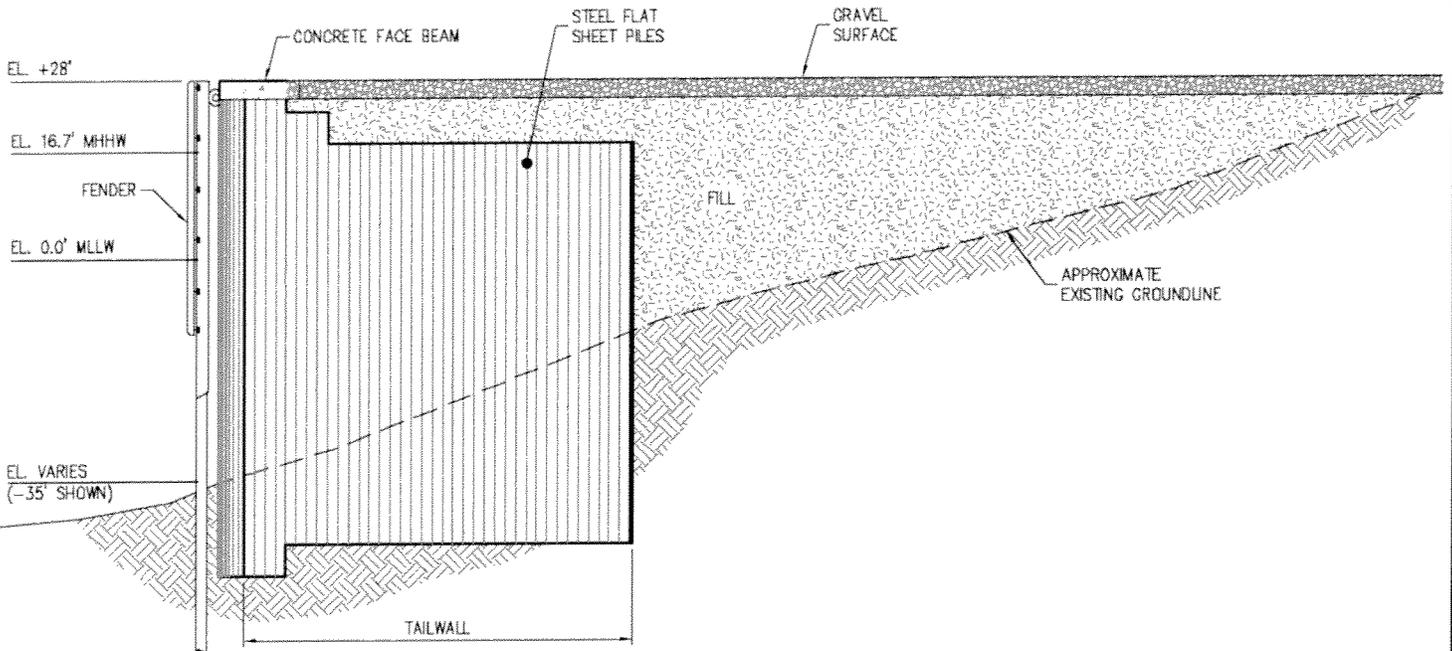
NEW BULKHEAD DOCK

APPLICATION BY:
MUNICIPALITY OF SKAGWAY
P.O. BOX 415
SKAGWAY, ALASKA 99840

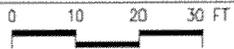
PROPOSED: CONSTRUCT IN PHASES, A 50'x340' STEEL PILE SUPPORTED, CONCRETE DECK DOCK, 800LF OF SHEET PILE BULKHEAD, MOORING AND BREASTING DOLPHINS AND NEW ORE LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
Sec. 14, Twp 28 S, R 59 E

SHEET 11 of 17 DATE: JUNE 2010



SECTION A-A



PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.
DATUM: 0.0' MLLW
ADJACENT PROPERTY OWNERS:
 1. SKAGWAY TERMINAL CO.
 (RAILROAD RIGHT-OF-WAY)
 2. MUNICIPALITY OF SKAGWAY
 3. STATE OF ALASKA

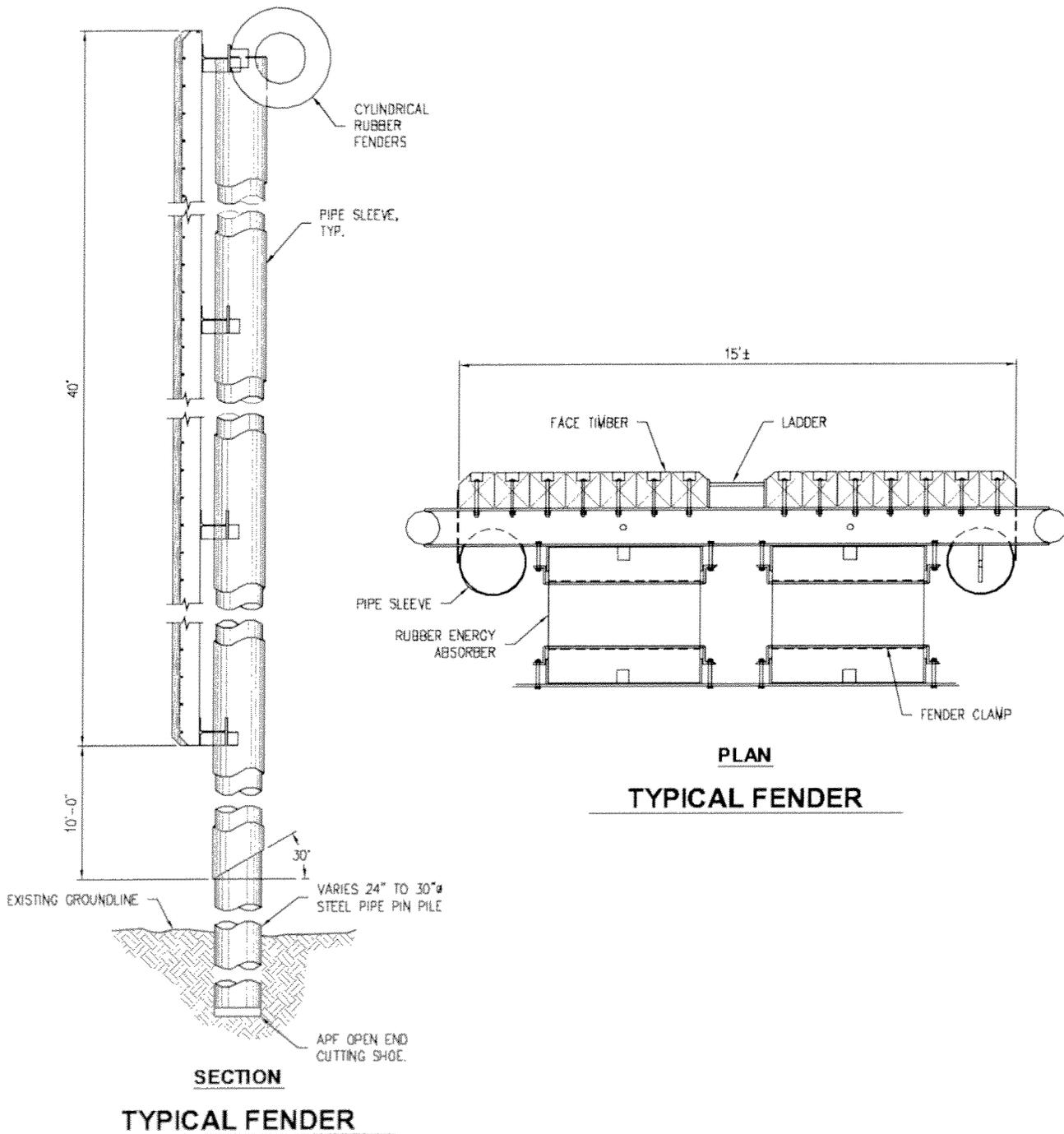
**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 1
 NEW BULKHEAD DOCK**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 12 of 17 DATE: JUNE 2010

Drawings \2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - Pt.3.dwg



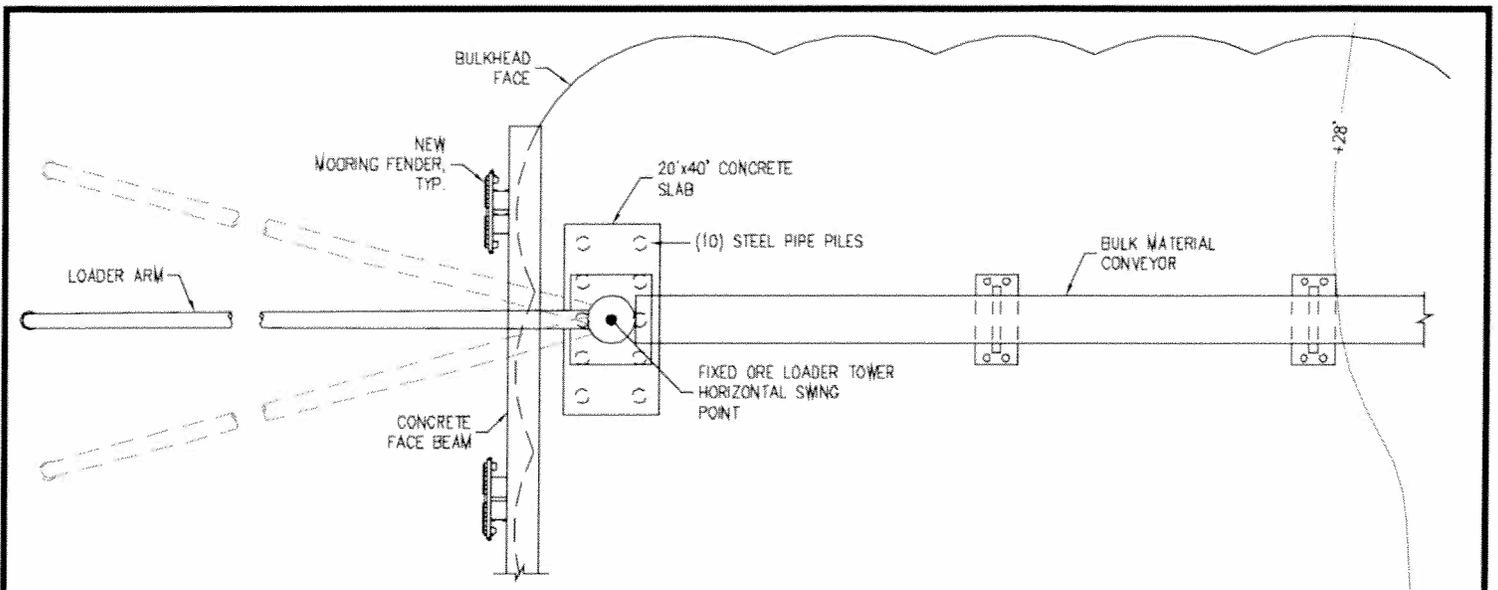
PURPOSE.
 PROVIDE INTERMODAL PORT FACILITIES FOR PASSENGERS AND LOOSE CARGO.
DATUM. 0.0' MLLW
ADJACENT PROPERTY OWNERS:
 1. SKAGWAY TERMINAL CO.
 (RAILROAD RIGHT-OF-WAY)
 2. MUNICIPALITY OF SKAGWAY
 3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 NEW FENDERS**

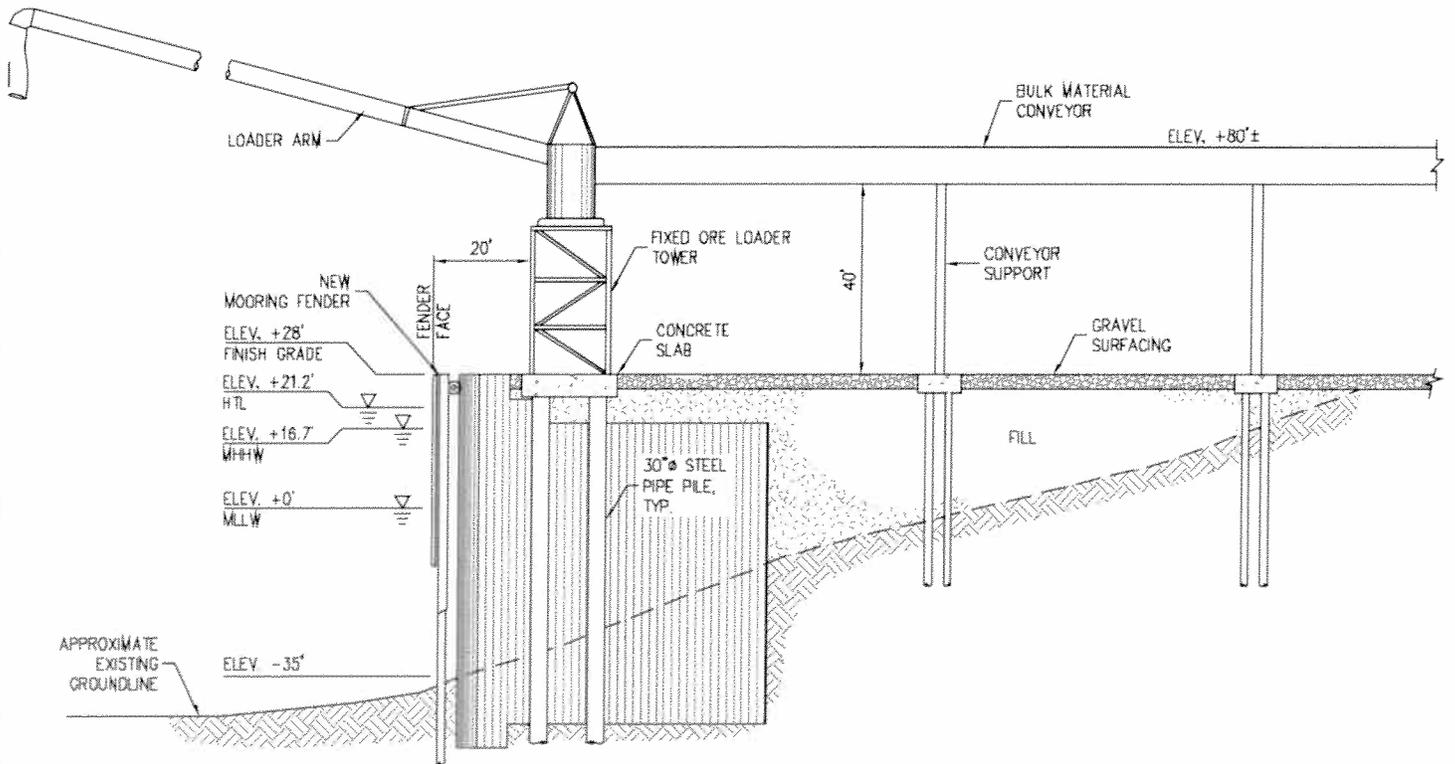
APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 13 of 17 DATE: JUNE 2010



PLAN



SECTION

PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

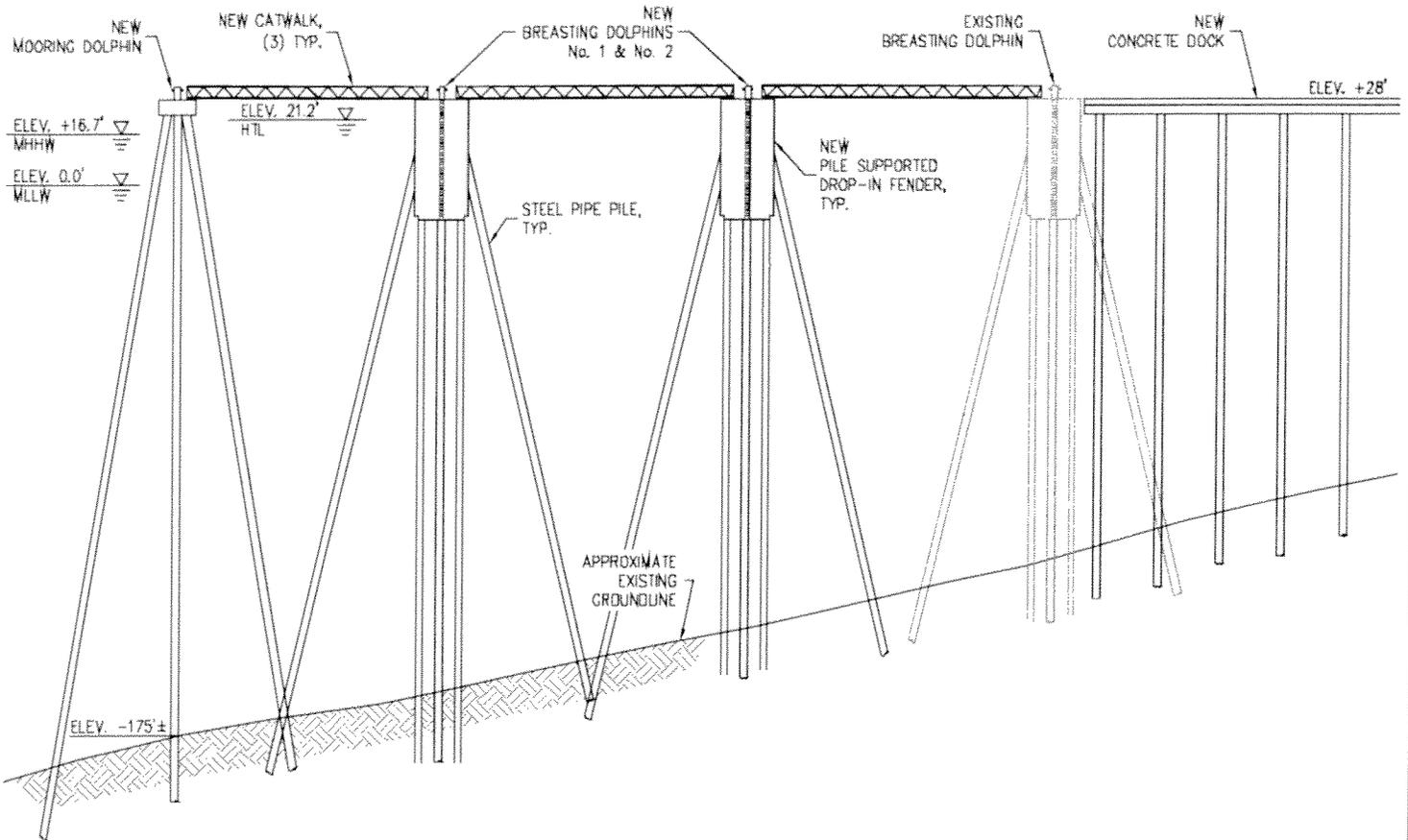
**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 1
 NEW ORE LOADER**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
 AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 10 of 17 DATE: JUNE 2010

Drawings\2009\094050.01 - Skagway Gateway Project\Permit 2010\094050.01 - P14.dwg



ELEVATION



PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

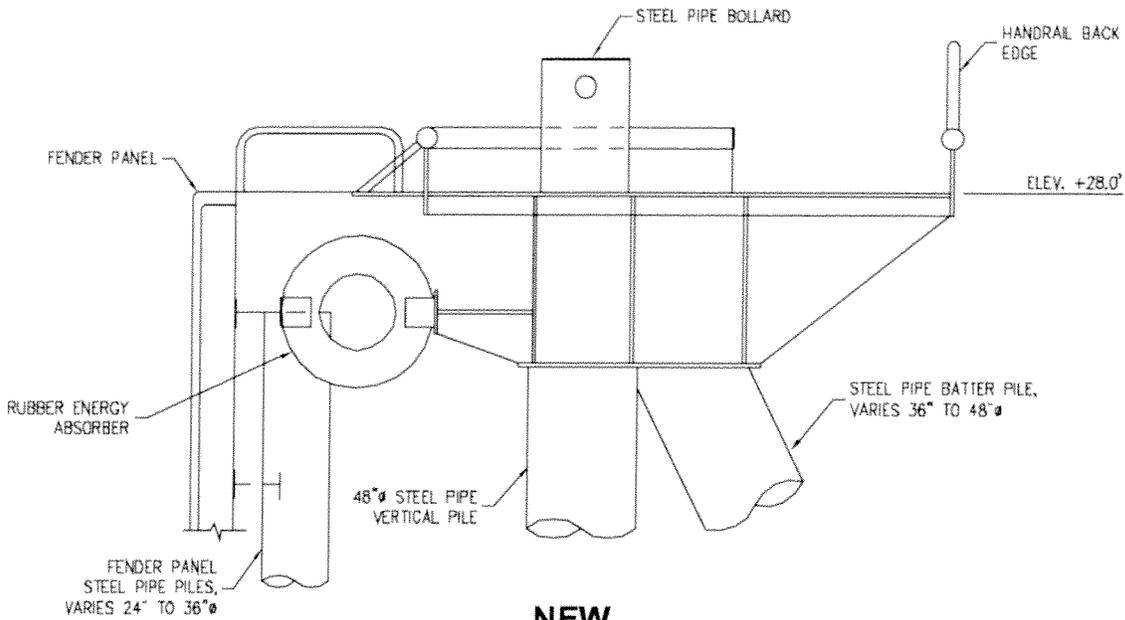
1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 2
 NEW CONCRETE DOCK
 DOLPHINS**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

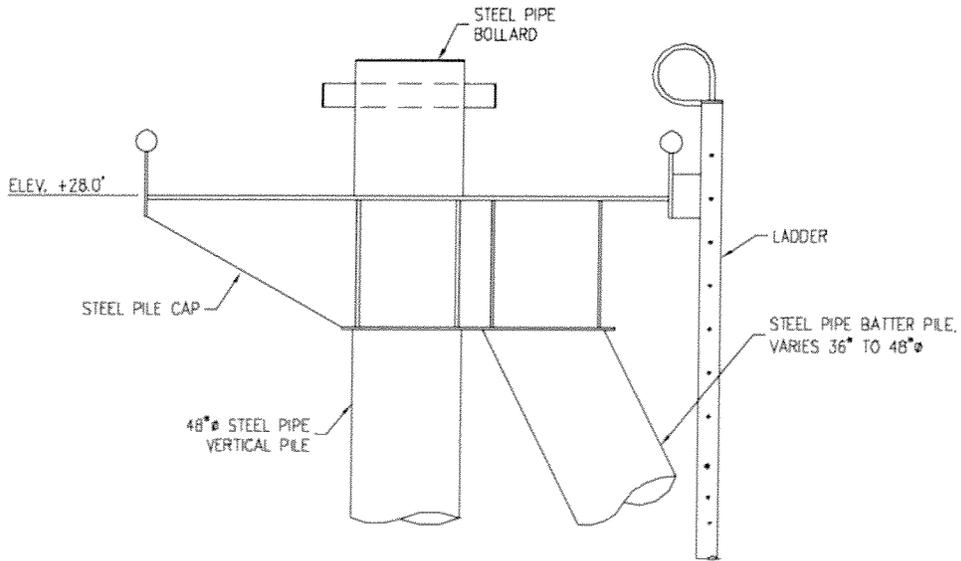
PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E
SHEET 15 of 17 DATE: JUNE 2010



**NEW
BREASTING DOLPHIN**

NOT TO SCALE



**NEW
MOORING DOLPHIN**

PURPOSE:
PROVIDE INTERMODAL
PORT FACILITIES FOR PASSENGERS
AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS:

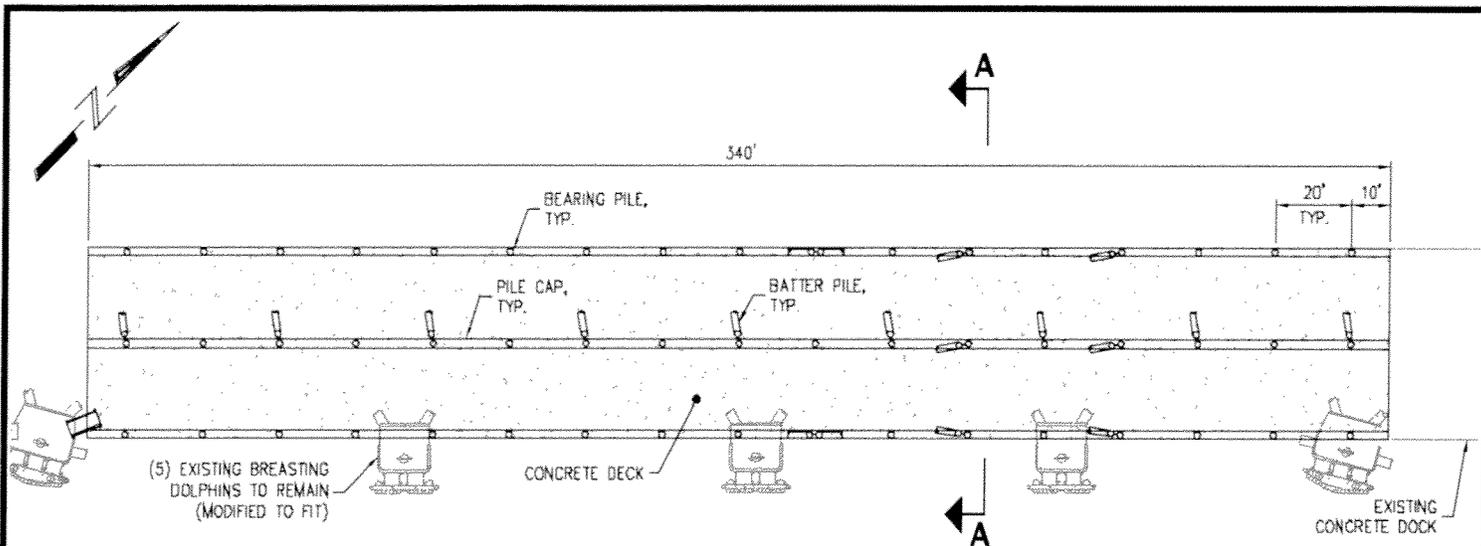
1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
SKAGWAY, ALASKA
PHASE 2
NEW CONCRETE DOCK
DOLPHINS**

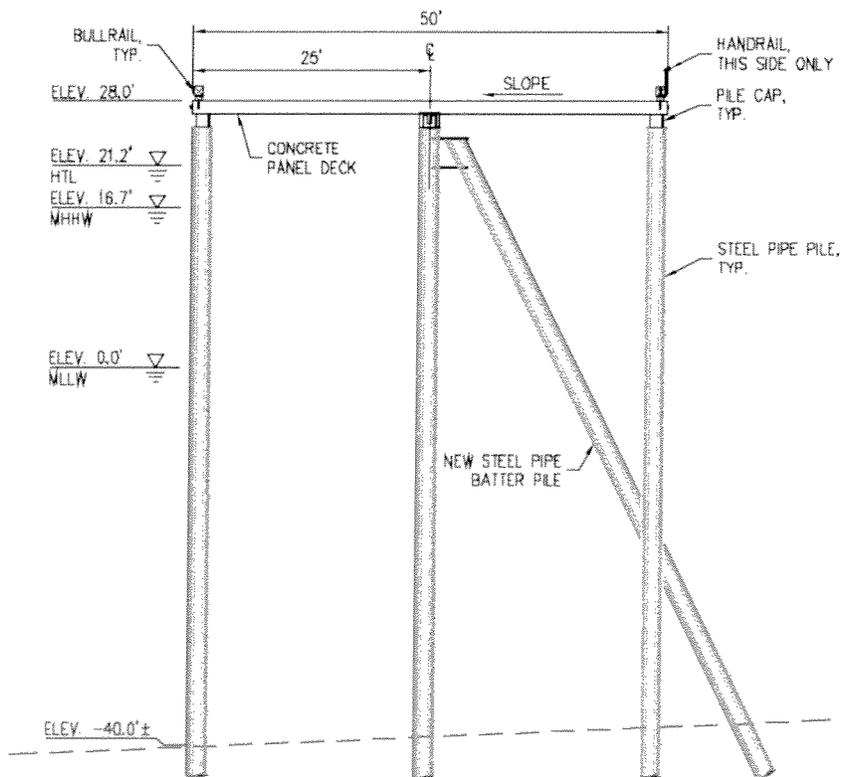
APPLICATION BY:
MUNICIPALITY OF SKAGWAY
P.O. BOX 415
SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
A 50'x340' STEEL PILE SUPPORTED,
CONCRETE DECK DOCK, 800LF OF
SHEET PILE BULKHEAD, MOORING AND
BREASTING DOLPHINS AND NEW ORE
LOADER.

IN: TAIYA INLET
AT: SKAGWAY, ALASKA
Sec. 14, Twp 28 S, R 59 E
SHEET 16 of 17 DATE: JUNE 2010



PLAN



SECTION A-A

Drawings\2009\09-4050.01 - Skagway Gateway Project\Permit 2010\09-4050.01--P17.dwg

PURPOSE:
 PROVIDE INTERMODAL
 PORT FACILITIES FOR PASSENGERS
 AND LOOSE CARGO.

DATUM: 0.0' MLLW

ADJACENT PROPERTY OWNERS.

1. SKAGWAY TERMINAL CO.
(RAILROAD RIGHT-OF-WAY)
2. MUNICIPALITY OF SKAGWAY
3. STATE OF ALASKA

**YUKON GATEWAY REVISED
 SKAGWAY, ALASKA
 PHASE 2
 NEW CONCRETE DOCK**

APPLICATION BY:
 MUNICIPALITY OF SKAGWAY
 P.O. BOX 415
 SKAGWAY, ALASKA 99840

PROPOSED: CONSTRUCT IN PHASES,
 A 50'x340' STEEL PILE SUPPORTED,
 CONCRETE DECK DOCK, 800LF OF
 SHEET PILE BULKHEAD, MOORING AND
 BREASTING DOLPHINS AND NEW ORE
 LOADER.

IN: TAIYA INLET
 AT. SKAGWAY, ALASKA
 Sec. 14, Twp 28 S, R 59 E

SHEET 17 of 17 DATE: JUNE 2010

SEAN PARNELL, GOVERNOR

STATE OF ALASKA

DEPARTMENT OF NATURAL RESOURCES
DIVISION OF COASTAL AND OCEAN MANAGEMENT

DIVISION OF COASTAL AND OCEAN MANAGEMENT
POST OFFICE BOX 111030, MS 1030
JUNEAU, ALASKA 99811-1030
PHONE: (907) 465-3562/FAX: (907) 465-3075

NOTICE OF APPLICATION
FOR
CERTIFICATION OF CONSISTENCY WITH THE
ALASKA COASTAL MANAGEMENT PROGRAM

Notice is hereby given that a request is being filed with the Division of Coastal and Ocean Management for a consistency determination, as provided in Section 307(c)(3) of the Coastal Zone Management Act of 1972, as amended [16 U.S.C. 1456(c)(3)], that the project described in the Corps of Engineers Public Notice No. POA-2009-1049, Taiya Inlet, will comply with the Alaska Coastal Management Program and that the project will be conducted in a manner consistent with that program.

This project is being reviewed for consistency with the Alaska Coastal Management Program. Written comments about the consistency of the project with the applicable ACMP statewide standards and district policies must be submitted to the Division of Coastal and Ocean Management (DCOM). For information about this consistency review, contact DCOM at the address or phone number above, or visit the ACMP web site at <http://www.alaskacoast.state.ak.us//Projects/projects.html>.

SEAN PARNELL, GOVERNOR

STATE OF ALASKA

DEPT. OF ENVIRONMENTAL CONSERVATION
DIVISION OF WATER
401 Certification Program
Non-Point Source Water Pollution Control Program

DEPARTMENT OF ENVIRONMENTAL CONSERVATION
WQM/401 CERTIFICATION
410 WILLOUGHBY AVENUE
JUNEAU, ALASKA 99801-1795
PHONE: (907) 465-5321/FAX: (907) 465-5274

NOTICE OF APPLICATION FOR STATE WATER QUALITY CERTIFICATION

Any applicant for a federal license or permit to conduct an activity that might result in a discharge into navigable waters, in accordance with Section 401 of the Clean Water Act of 1977 (PL95-217), also must apply for and obtain certification from the Alaska Department of Environmental Conservation that the discharge will comply with the Clean Water Act, the Alaska Water Quality Standards, and other applicable State laws. By agreement between the U.S. Army Corps of Engineers and the Department of Environmental Conservation, application for a Department of the Army permit to discharge dredged or fill material into navigable waters under Section 404 of the Clean Water Act also may serve as application for State Water Quality Certification.

Notice is hereby given that the application for a Department of the Army Permit described in the Corps of Engineers' Public Notice No. POA-2009-1049, Taiya Inlet, serves as application for State Water Quality Certification from the Department of Environmental Conservation.

After reviewing the application, the Department may certify there is reasonable assurance the activity, and any discharge that might result, will comply with the Clean Water Act, the Alaska Water Quality Standards, and other applicable State laws. The Department also may deny or waive certification.

Any person desiring to comment on the project, with respect to Water Quality Certification, may submit written comments to the address above by the expiration date of the Corps of Engineer's Public Notice.